



## ADDENDUM NO. 3

Date: May 24, 2019  
Project: Cherokee Mental Health Institute  
Tunnel Repairs  
DAS RFB 0919335132  
DAS Project 9072.00

General Information: The Engineer's cost opinion is \$1,469,000.

See the attached "Questions from Bidders" document. (References made in the Document to Addendum 2 should actually be Addendum 3).

### DRAWING ITEMS:

1. **DS1.02** – Add "There is not a separate sidewalk on top of the Ginzberg tunnel lid. The tunnel lid is the current sidewalk."
2. **S1.03** – Modify exhaust fan replacement requirements.
3. **S1.04** – Modify exhaust fan replacement requirements.
4. **S1.04** – Add existing mechanical unit information.
5. **Detail 2/S1.11** – Delete "Steel Grate" note.
6. **Detail 2/S1.12** – Add brick requirements.
7. **Detail 2/S1.12** – Add alternate bond beam option for CMU installation.

### SPECIFICATION ITEMS:

1. SECTION 00 3143 – PERMIT APPLICATIONS
  - a. ADD Section 1.02.E as follows, "E. Trade Contractor shall be responsible for all NPDES requirements, including development of storm water pollution prevention plan, permit, erosion control measures, inspections, and termination of the permit.

### ATTACHMENTS:

1. Questions from Bidders
2. Revised Sheet DS1.02
3. Revised Sheet S1.03
4. Revised Sheet S1.04
5. Revised Sheet S1.11
6. Revised Sheet S1.12

END OF ADDENDUM



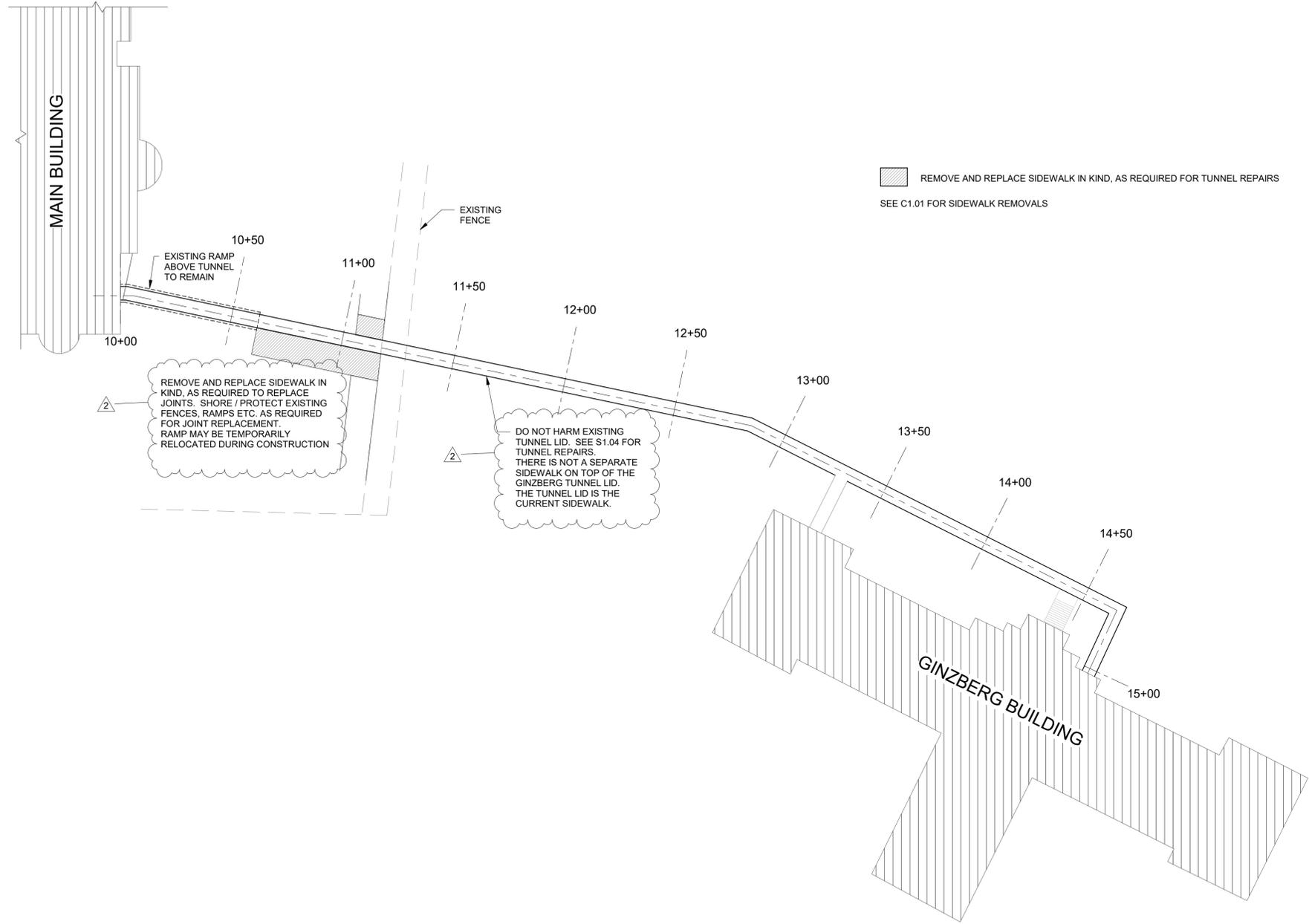
Questions from Bidders (SBI responses in blue):

1. I'd like to know the size of the condensing unit were are removing & reinstalling. Existing York R22 15 ton. Approx. 9'-2"x3'-8"x3'-2" 1600 LBS. See Addendum 2.
2. Specs on the fan: CFM, voltage, horse power, brand etc.. Minimum: 1850 CFM, 115 VOLTS, ¼ H.P., 1500 RPM, no specific brand. See Addendum 2.
3. Specs on the Louver or damper, color, brand etc.. Louver to be stainless steel, galvanize, or powder coated gray. Louver to be gravity working. No specific brand. See Addendum 2.
4. Will the owner be paying for the NPDES Permit #2.
5. Sheet S1.04 calls out for (3) tunnel joints to be replaced in the CCUSO yard but the ramp is not to be removed, there will also be holes next to the walks 7' deep x7' wide x6' long on each side for the work. Ramp may be temporarily relocated during construction.
6. Sheet S1.03 call out Replace existing exhaust fan with new at the Voldeng Hall corner, on the tour the only fan was at the Ginzberg tunnel entrance also shown on Sheet S1.04. The fan is located in a fenced in room at the end of the tunnel and was shown at the end of the visit.
7. Sheet S1.12 Detail 2 shows rebar tied at the top and bottom this is not achievable, we can tie the bottom and tie the top using a bond beam at the top with rebar doweled in both sides. The detail is achievable. The reinforcing bars can be spliced. A bottom dowel can be installed to the existing bottom bricks with a 10" gap at the top to allow for CMU to be dropped down. A top dowel bar can be tied to the lower bottom bar. The holes for the top dowel can be drilled. All but the top course of CMU can be installed. Then remove the dowel ties and install the top dowels. Remove the faces of one side of the CMU for installation and grouting. A bond beam is an acceptable alternate, see Addendum 2.
8. Sheet S1.11 Detail 2 calls out sump pit grate, is there a manufacturer or brand, is this round, what is the thickness required. Does it have to be aluminum, there are hundreds of premade steel, poly & plastic grates and frames available made for sump pits. Grating to be aluminum per the plans.
9. Has a brick been picked for the window infills what allowance for brick should be used. See Addendum 2. Match color range, texture, size, and coursing of existing adjacent brick work. Brick to meet ASTM C216, Type FBS, Grade SW. Color match to be no further than the differences at the existing window infills. Allowances are not provided.
10. Sheet DS1.02 only show the in-kind sidewalk removal, Sheet DS1.01 shows the sidewalk removal at the tunnels, shouldn't there be total replacement at the waterproofing locations or are they only waterproofing the sides. See Addendum 2. There is not an existing sidewalk on top of the tunnel at Ginzberg. The tunnel lid is currently used as the sidewalk. The tunnel lid is not to be harmed.
11. Do to the joint repair in the CCUSO yard there will be multiple dirt piles and we will have to came back after back fill and seed the area, please note this as a scheduling item.

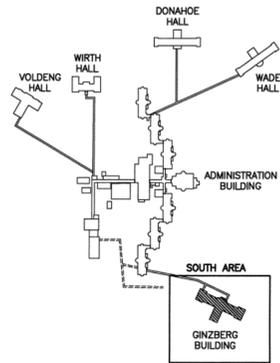
GINZBERG BUILDING TUNNEL - DEMO PLAN

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1 GINZBERG BUILDING TUNNEL AT-GRADE DEMOLITION PLAN  
1" = 30'-0"



SOUTH KEY PLAN

MARK	REVISION	DATE	BY
2	ADDENDUM 3	5/24/19	
Engineer:	BDM	Checked By:	CGG
Technician:	DBT	Date:	4/19/19
Field Bk:		Scale:	(AS NOTED)
Project No:	119.0096.03	Pg:	Sheet DS1.02

9072.00 DHS CHMHI TUNNEL REPAIRS  
CHEROKEE, IOWA

GINZBERG BUILDING TUNNEL - DEMO PLAN  
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515-243-4477  
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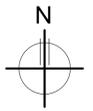
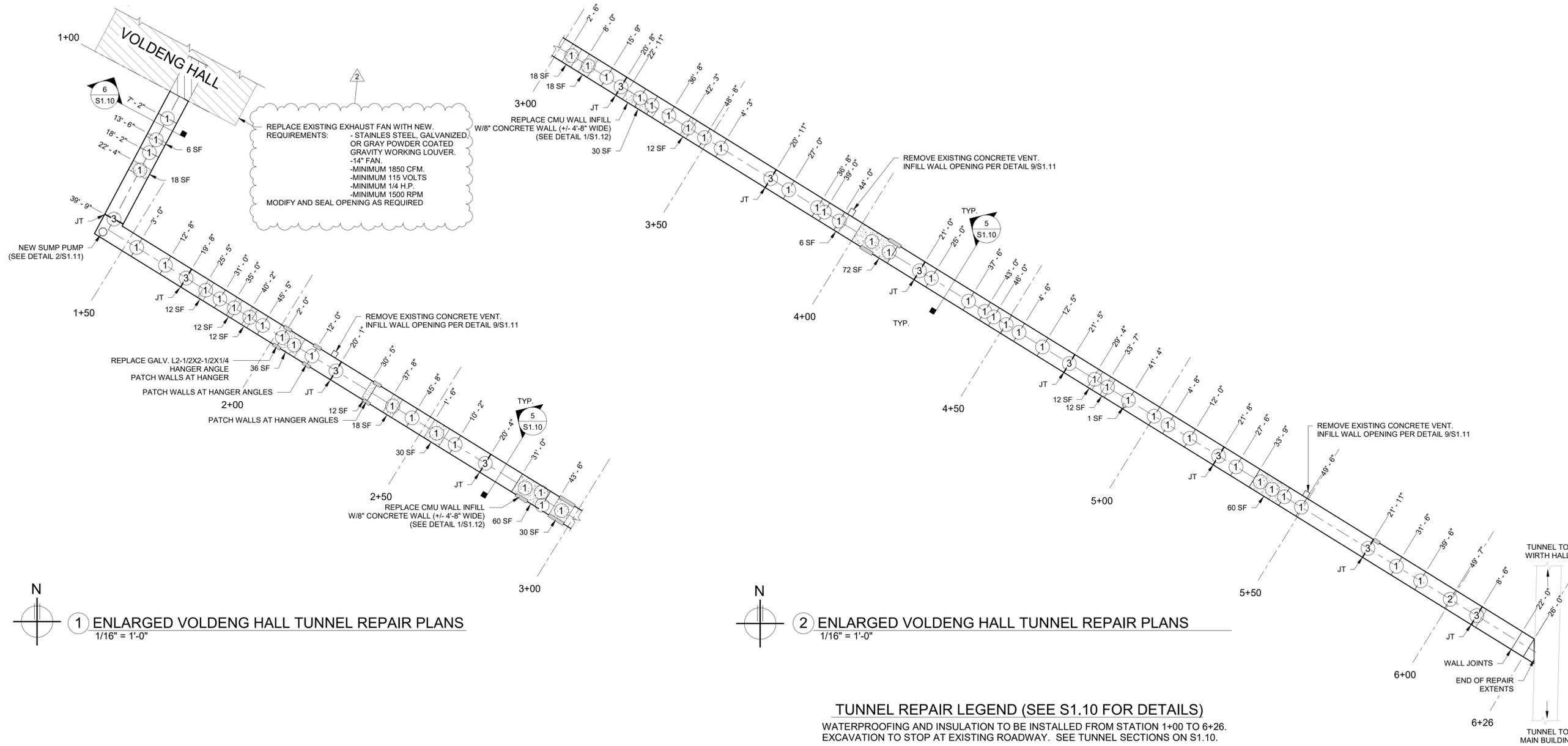
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Project No: 119.0096.03  
Sheet DS1.02

VOLDENG HALL TUNNEL - ENLARGED PLANS

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1 ENLARGED VOLDENG HALL TUNNEL REPAIR PLANS  
1/16" = 1'-0"



2 ENLARGED VOLDENG HALL TUNNEL REPAIR PLANS  
1/16" = 1'-0"

**TUNNEL REPAIR LEGEND (SEE S1.10 FOR DETAILS)**

WATERPROOFING AND INSULATION TO BE INSTALLED FROM STATION 1+00 TO 6+26.  
EXCAVATION TO STOP AT EXISTING ROADWAY. SEE TUNNEL SECTIONS ON S1.10.

- 1 CRACK INJECTION (WALLS AND LID) (SEE DETAIL 3/S1.10)
- 2 CRACK INJECTION (WALLS ONLY) (SEE DETAIL 3/S1.10)
- 3 JOINT REPLACEMENT (WALLS AND LID) (SEE DETAIL 4/S1.11)

- PARTIAL DEPTH CONCRETE REPAIR (UNDERSIDE OF LID) (SEE DETAIL 1/S1.10)
- PARTIAL DEPTH CONCRETE REPAIR (INTERIOR OF WALL) (SEE DETAIL 1/S1.10)
- RESURFACING (BOTTOM OF LID) (SEE DETAIL 4/S1.10)
- REPLACE TUNNEL LID (SEE DETAIL 7/S1.10)

INSTALL NEW SUBDRAINS PER DETAILS ON S1.10 & ELEVATIONS ON CIVIL DRAWINGS  
PROTECT IN PLACE ALL UTILITIES, LIGHT POLES, ETC. AS REQUIRED DURING CONSTRUCTION.

2	ADDENDUM 3	5/24/19	DATE	BY
MARK	REVISION	DATE	BY	(AS NOTED)
Engineer: BDM	Checked By: CGG	Scale:	(AS NOTED)	
Technician: DBT	Date: 4/19/19	Field Bk:	Pg:	
Project No: 119.0096.03	Sheet S1.03			

**9072.00 DHS CHMHI TUNNEL REPAIRS**

**VOLDENG HALL TUNNEL - ENLARGED PLANS**

**CHEROKEE, IOWA**

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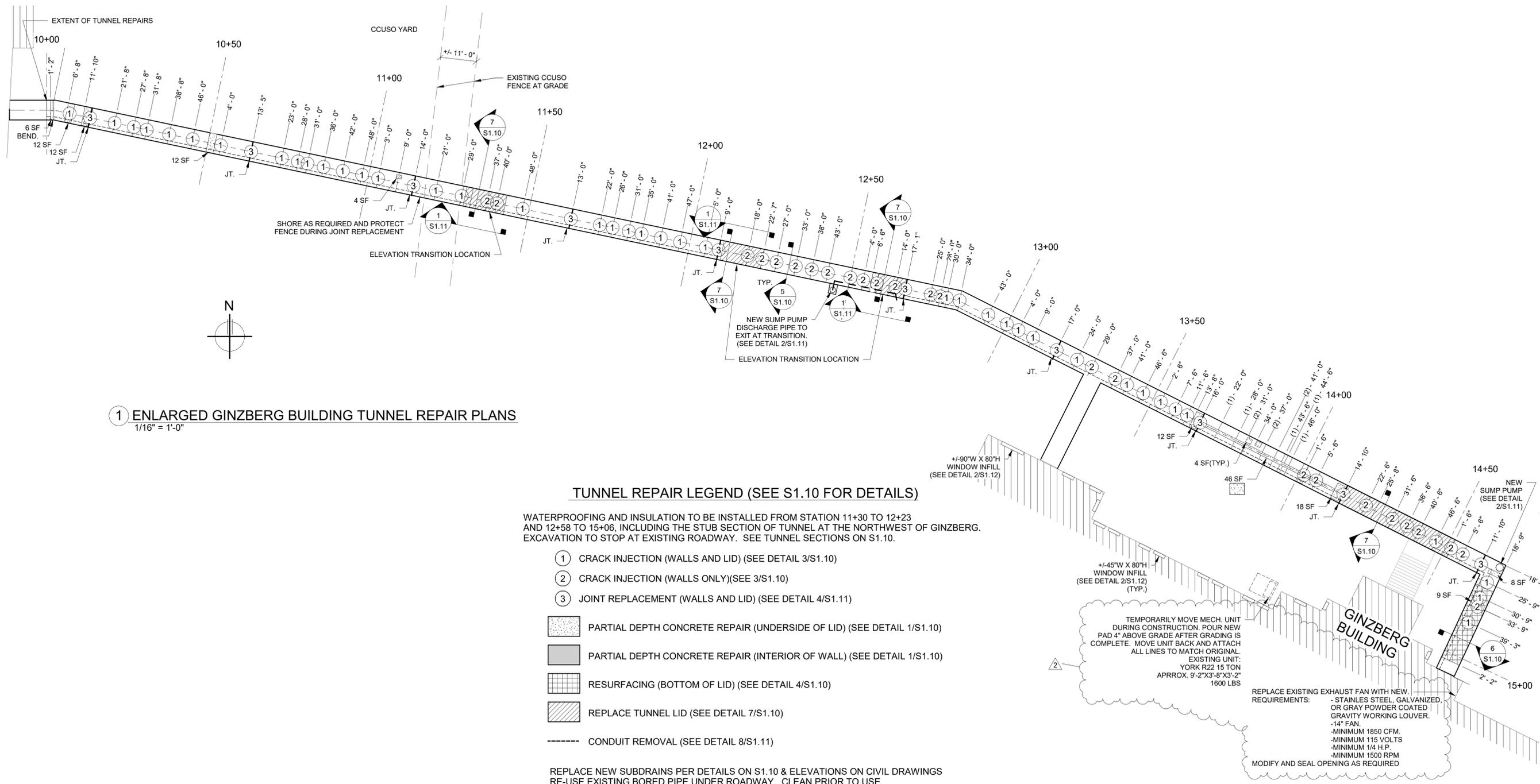
Project No: 119.0096.03

Sheet S1.03

**CCUSO YARD CONSTRUCTION NOTES**

1. WORK WITH CCUSO YARD TO BE COMPLETED IN 3 WEEKS
2. COORDINATE ALL EQUIPMENT, ENTRY TIMES, AND EXIT TIMES WITH CCUSO.
3. WHILE WORKING IN YARD NO. OTHER OPENINGS IN THE TUNNEL ARE PERMITTED TO PREVENT POSSIBLE ESCAPE ROUTES
4. CONTRACTOR TO TEMPORARILY SEAL ANY HOLES IN TUNNEL AT THE END OF EACH WORK DAY.
5. CONTRACTOR TO REMOVE ALL TOOLS AND EQUIPMENT AT THE END OF EACH WORK DAY.
6. COORDINATE ALL WORK AND SECURITY WITH CCUSO.
7. WORK LIMITS WITHIN CCUSO YARD ARE 8:00 AM - 4:30 PM.
8. SHORE / PROTECT EXISTING FENCES, RAMPS, ETC. AS REQUIRED DURING JOINT REPLACEMENT.

GINZBERG BUILDING TUNNEL - ENLARGE PLANS



**1 ENLARGED GINZBERG BUILDING TUNNEL REPAIR PLANS**  
1/16" = 1'-0"

**TUNNEL REPAIR LEGEND (SEE S1.10 FOR DETAILS)**

WATERPROOFING AND INSULATION TO BE INSTALLED FROM STATION 11+30 TO 12+23 AND 12+58 TO 15+06, INCLUDING THE STUB SECTION OF TUNNEL AT THE NORTHWEST OF GINZBERG. EXCAVATION TO STOP AT EXISTING ROADWAY. SEE TUNNEL SECTIONS ON S1.10.

- 1 CRACK INJECTION (WALLS AND LID) (SEE DETAIL 3/S1.10)
- 2 CRACK INJECTION (WALLS ONLY)(SEE 3/S1.10)
- 3 JOINT REPLACEMENT (WALLS AND LID) (SEE DETAIL 4/S1.11)
- PARTIAL DEPTH CONCRETE REPAIR (UNDERSIDE OF LID) (SEE DETAIL 1/S1.10)
- PARTIAL DEPTH CONCRETE REPAIR (INTERIOR OF WALL) (SEE DETAIL 1/S1.10)
- RESURFACING (BOTTOM OF LID) (SEE DETAIL 4/S1.10)
- REPLACE TUNNEL LID (SEE DETAIL 7/S1.10)
- CONDUIT REMOVAL (SEE DETAIL 8/S1.11)

REPLACE NEW SUBDRAINS PER DETAILS ON S1.10 & ELEVATIONS ON CIVIL DRAWINGS RE-USE EXISTING BORED PIPE UNDER ROADWAY. CLEAN PRIOR TO USE.

PROTECT IN PLACE ALL UTILITIES, LIGHT POLES, ETC. AS REQUIRED DURING CONSTRUCTION.

TEMPORARILY MOVE MECH. UNIT DURING CONSTRUCTION. POUR NEW PAD 4" ABOVE GRADE AFTER GRADING IS COMPLETE. MOVE UNIT BACK AND ATTACH ALL LINES TO MATCH ORIGINAL. EXISTING UNIT: YORK R22 15 TON APPROX. 9'-2"x3'-8"x3'-2" 1600 LBS

REPLACE EXISTING EXHAUST FAN WITH NEW. REQUIREMENTS:  
 - STAINLESS STEEL, GALVANIZED, OR GRAY POWDER COATED GRAVITY WORKING LOUVER.  
 - 14" FAN.  
 - MINIMUM 1850 CFM.  
 - MINIMUM 115 VOLTS  
 - MINIMUM 1/4 H.P.  
 - MINIMUM 1500 RPM  
 MODIFY AND SEAL OPENING AS REQUIRED

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2	ADDENDUM 3	5/24/19	DATE	BY
1	ADDENDUM 1	5/15/19	DATE	BY
MARK	REVISION	CGG	Scale:	(AS NOTED)
Engineer:	BDM	Checked By:	CGG	Scale:
Technician:	DBT	Date:	4/19/19	Field Bk:
Project No.:	119.0096.03	Pg.:		Sheet S1.04

**9072.00 DHS CHMHI TUNNEL REPAIRS**

**GINZBERG BUILDING TUNNEL - ENLARGE PLANS**

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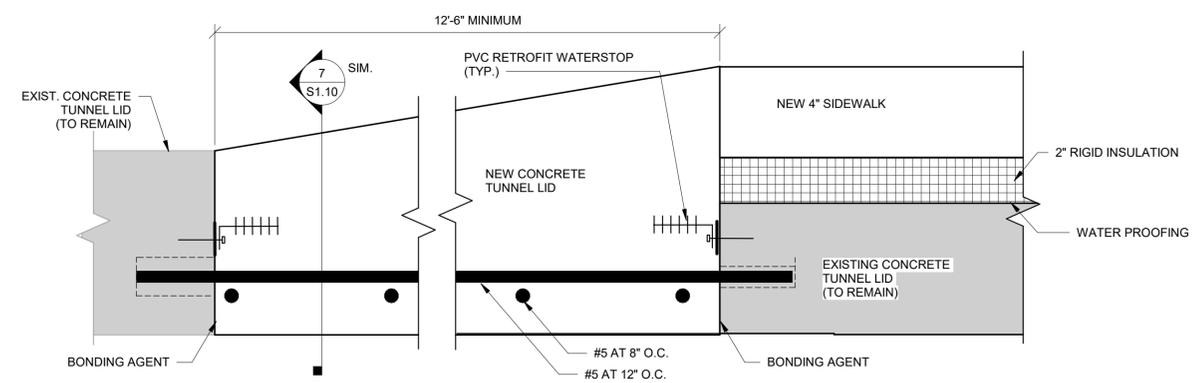
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Sheet S1.04

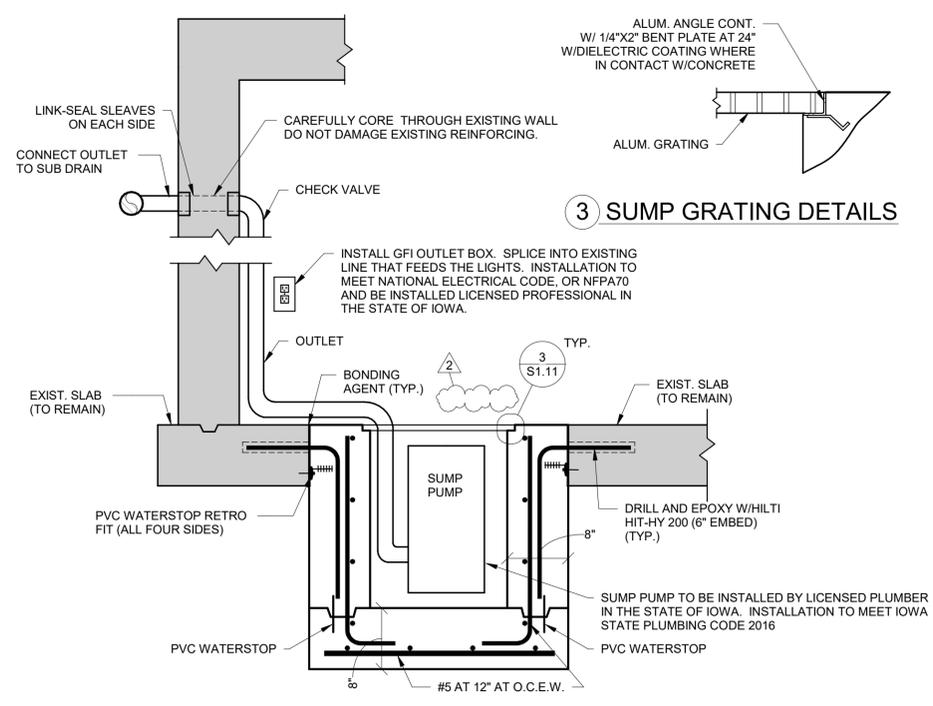
DETAILS

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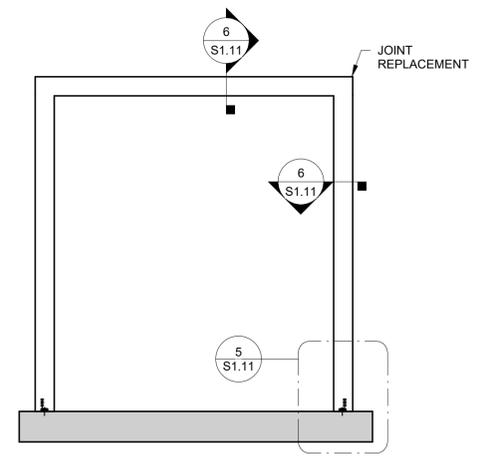
1 CONCRETE TUNNEL LID TRANSITION  
3" = 1'-0"



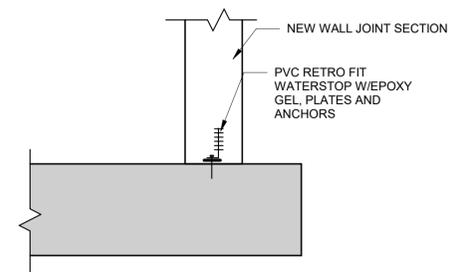
3 SUMP GRATING DETAILS

NOTE: EACH PUMP SHALL BE RATED A MINIMUM 1/2 H.P., 115 VOLTS, 1 PHASE, 60 HERTZ AND OPERATE AT 3450 RPM.

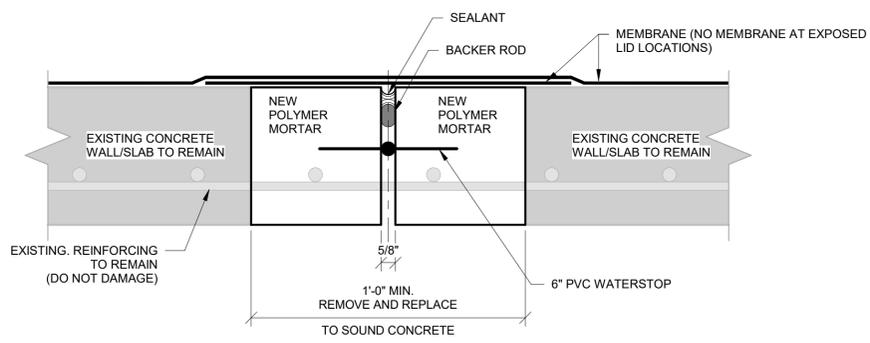
2 TYPICAL SUMP PIT DETAIL  
1" = 1'-0"



4 JOINT REPLACEMENT  
1/2" = 1'-0"

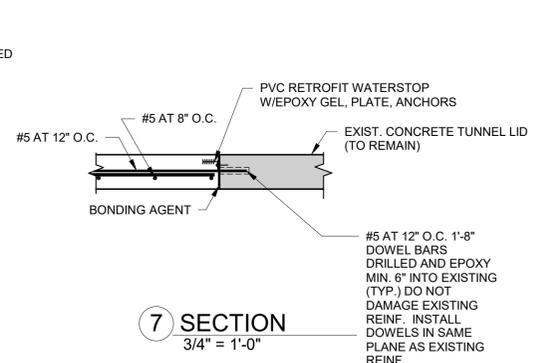


5 SECTION  
1 1/2" = 1'-0"

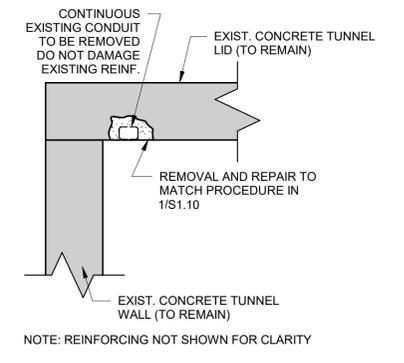


NOTE: PROTECT AND DO NOT HARM ANY EXISTING UTILITIES

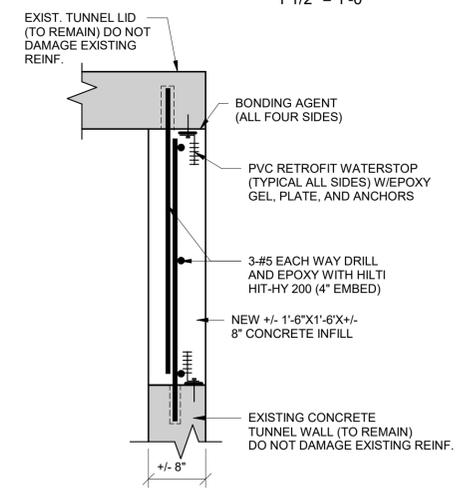
6 SECTION  
3" = 1'-0"



7 SECTION  
3/4" = 1'-0"



8 TYPICAL CONDUIT REMOVAL DETAIL  
1 1/2" = 1'-0"



NOTE: EXIST. REINF. NOT SHOWN FOR CLARITY

9 TYPICAL VENT INFILL DETAIL  
1 1/2" = 1'-0"

Project No:	119.0096.03	Sheet:	S1.11
Field Bk:		Scale:	(AS NOTED)
Technician:	DBT	Date:	4/19/19
Engineer:	BDM	Checked By:	CGG
MARK	REVISION	DATE	BY
2	ADDENDUM 3	5/24/19	

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DETAILS

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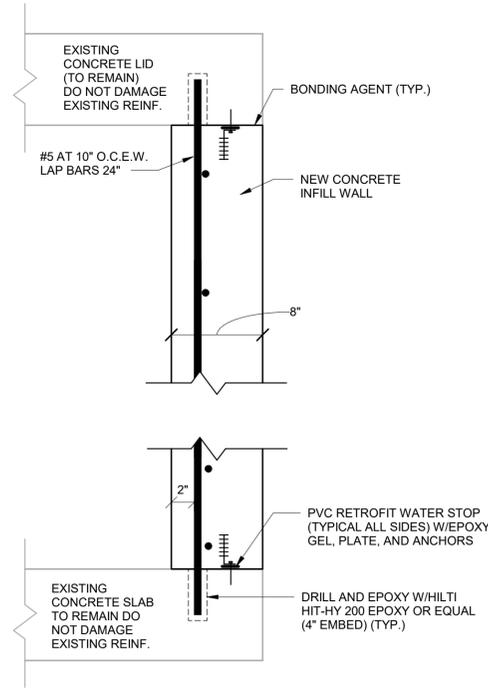
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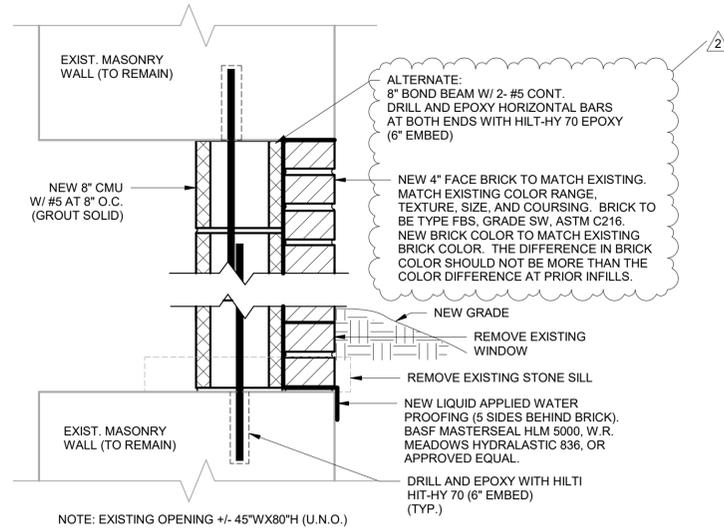
Project No: 119.0096.03

Sheet S1.11

DETAILS



1 TYPICAL WALL INFILL DETAIL  
1 1/2" = 1'-0"



2 TYPICAL WINDOW INFILL DETAIL  
1 1/2" = 1'-0"

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2	ADDENDUM 3	5/24/19	DATE	BY	CGG	Scale:	(AS NOTED)
1	ADDENDUM 1	5/15/19	DATE	BY	BDM	Checked By:	(AS NOTED)
MARK		REVISION	DATE	BY	CGG	Scale:	(AS NOTED)
Engineer:	BDM	Checked By:	CGG	Scale:	(AS NOTED)	Field Bk:	Pg:
Technician:	DBT	Date:	4/19/19	Field Bk:	Pg:	119.0096.03	Sheet S1.12

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Sheet S1.12