



ADDENDUM NO. 3

Date: May 24, 2019

Project: Cherokee Mental Health Institute
Tunnel Repairs
DAS RFB 0919335132
DAS Project 9072.00

General Information: The Engineer's cost opinion is \$1,469,000.

See the attached "Questions from Bidders" document. (References made in the Document to Addendum 2 should actually be Addendum 3).

DRAWING ITEMS:

1. **DS1.02** – Add "There is not a separate sidewalk on top of the Ginzberg tunnel lid. The tunnel lid is the current sidewalk."
2. **S1.03** – Modify exhaust fan replacement requirements.
3. **S1.04** – Modify exhaust fan replacement requirements.
4. **S1.04** – Add existing mechanical unit information.
5. **Detail 2/S1.11** – Delete "Steel Grate" note.
6. **Detail 2/S1.12** – Add brick requirements.
7. **Detail 2/S1.12** – Add alternate bond beam option for CMU installation.

SPECIFICATION ITEMS:

1. SECTION 00 3143 – PERMIT APPLICATIONS
 - a. ADD Section 1.02.E as follows, "E. Trade Contractor shall be responsible for all NPDES requirements, including development of storm water pollution prevention plan, permit, erosion control measures, inspections, and termination of the permit."

ATTACHMENTS:

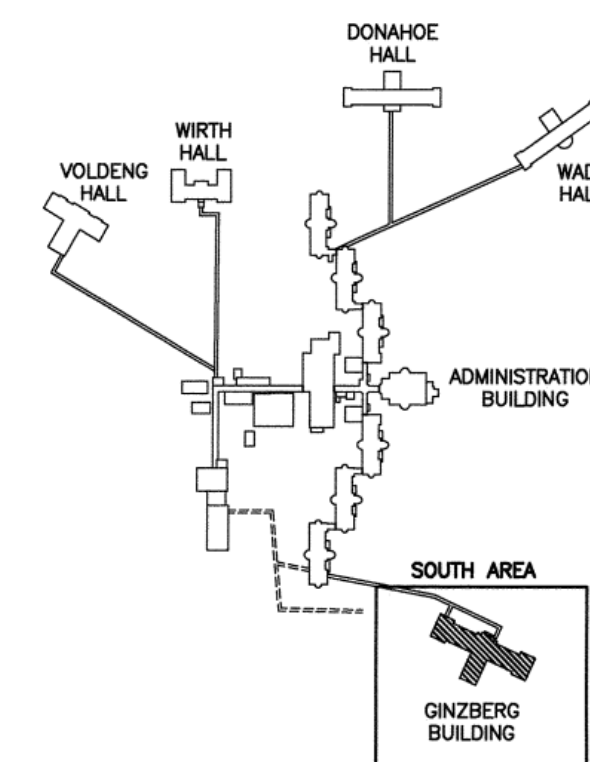
1. Questions from Bidders
2. Revised Sheet DS1.02
3. Revised Sheet S1.03
4. Revised Sheet S1.04
5. Revised Sheet S1.11
6. Revised Sheet S1.12

END OF ADDENDUM



Questions from Bidders (SBI responses in blue):

1. I'd like to know the size of the condensing unit were are removing & reinstalling. Existing York R22 15 ton. Approx. 9'-2"x3'-8"x3'-2" 1600 LBS. See Addendum 2.
2. Specs on the fan: CFM, voltage, horse power, brand etc.. Minimum: 1850 CFM, 115 VOLTS, ¼ H.P., 1500 RPM, no specific brand. See Addendum 2.
3. Specs on the Louver or damper, color, brand etc.. Louver to be stainless steel, galvanize, or powder coated gray. Louver to be gravity working. No specific brand. See Addendum 2.
4. Will the owner be paying for the NPDES Permit #2.
5. Sheet S1.04 calls out for (3) tunnel joints to be replaced in the CCUSO yard but the ramp is not to be removed, there will also be holes next to the walks 7' deep x7' wide x6' long on each side for the work. Ramp may be temporarily relocated during construction.
6. Sheet S1.03 call out Replace existing exhaust fan with new at the Voldeng Hall corner, on the tour the only fan was at the Ginzberg tunnel entrance also shown on Sheet S1.04. The fan is located in a fenced in room at the end of the tunnel and was shown at the end of the visit.
7. Sheet S1.12 Detail 2 shows rebar tied at the top and bottom this is not achievable, we can tie the bottom and tie the top using a bond beam at the top with rebar doweled in both sides. The detail is achievable. The reinforcing bars can be spliced. A bottom dowel can be installed to the existing bottom bricks with a 10" gap at the top to allow for CMU to be dropped down. A top dowel bar can be tied to the lower bottom bar. The holes for the top dowel can be drilled. All but the top course of CMU can be installed. Then remove the dowel ties and install the top dowels. Remove the faces of one side of the CMU for installation and grouting. A bond beam is an acceptable alternate, see Addendum 2.
8. Sheet S1.11 Detail 2 calls out sump pit grate, is there a manufacturer or brand, is this round, what is the thickness required. Does it have to be aluminum, there are hundreds of premade steel, poly & plastic grates and frames available made for sump pits. Grating to be aluminum per the plans.
9. Has a brick been picked for the window infills what allowance for brick should be used. See Addendum 2. Match color range, texture, size, and coursing of existing adjacent brick work. Brick to meet ASTM C216, Type FBS, Grade SW. Color match to be no further than the differences at the existing window infills. Allowances are not provided.
10. Sheet DS1.02 only show the in-kind sidewalk removal, Sheet DS1.01 shows the sidewalk removal at the tunnels, shouldn't there be total replacement at the waterproofing locations or are they only waterproofing the sides. See Addendum 2. There is not an existing sidewalk on top of the tunnel at Ginzberg. The tunnel lid is currently used as the sidewalk. The tunnel lid is not to be harmed.
11. Do to the joint repair in the CCUSO yard there will be multiple dirt piles and we will have to came back after back fill and seed the area, please note this as a scheduling item.



SOUTH KEY PLAN

1 GINZBERG BUILDING TUNNEL AT-GRADE DEMOLITION PLAN
1" = 30'-0"

[illegible]

9072.00 DHS CHMHI TUNNEL REPAIRS

GINZBERG BUILDING TUNNEL - DEMO PLAN

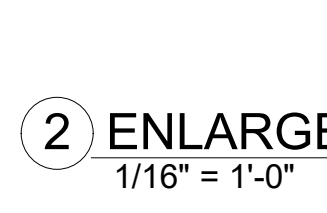
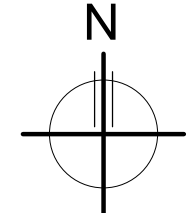
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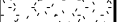
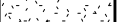

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Project No:	119.0096.03
Sheet	DS1.02



WATERPROOFING AND INSULATION TO BE INSTALLED FROM STATION 1+00 TO 6+26
EXCAVATION TO STOP AT EXISTING ROADWAY. SEE TUNNEL SECTIONS ON S1.10.

- | | |
|---|---|
|  | PARTIAL DEPTH CONCRETE REPAIR (UNDERSIDE OF LID) (SEE DETAIL 1/S1.10) |
|  | PARTIAL DEPTH CONCRETE REPAIR (INTERIOR OF WALL) (SEE DETAIL 1/S1.10) |
|  | RESURFACING (BOTTOM OF LID) (SEE DETAIL 4/S1.10) |
|  | REPLACE TUNNEL LID (SEE DETAIL 7/S1.10) |

INSTALL NEW SUBDRAINS PER DETAILS ON S1.10 & ELEVATIONS ON CIVIL DRAWINGS
PROTECT IN PLACE ALL UTILITIES, LIGHT POLES, ETC. AS REQUIRED DURING CONSTRUCTION

SBI SHUCK
BRITSON

Project No: 119.0096.03

Sheet S1.03

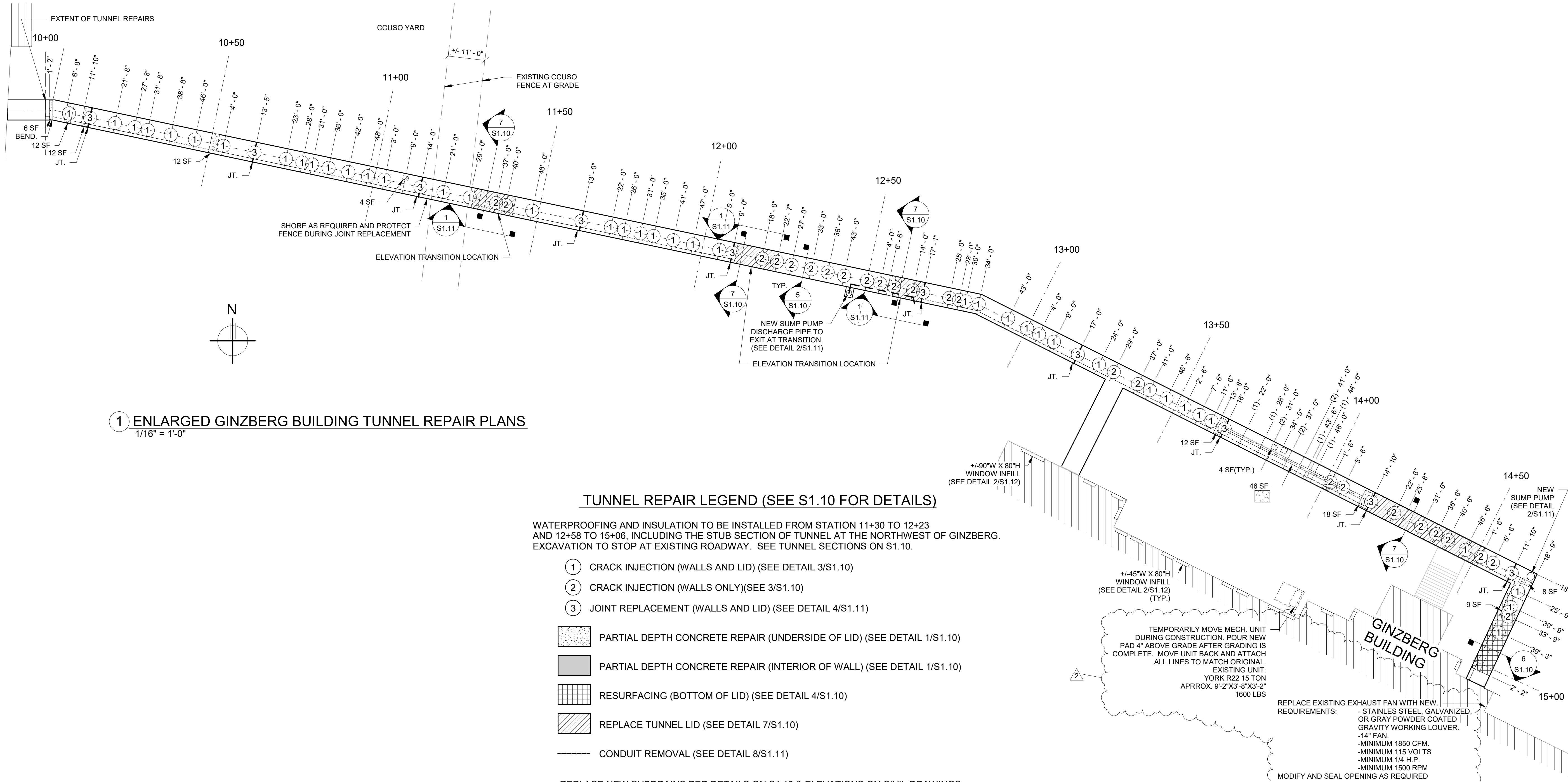
GINZBERG BUILDING TUNNEL - ENLARGE PLANS

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CCUSO YARD CONSTRUCTION NOTES

1. WORK WITH CCUSO YARD TO BE COMPLETED IN 3 WEEKS
2. COORDINATE ALL EQUIPMENT, ENTRY TIMES, AND EXIT TIMES WITH CCUSO.
3. WHILE WORKING IN YARD NO. OTHER OPENINGS IN THE TUNNEL ARE PERMITTED TO PREVENT POSSIBLE ESCAPE ROUTES
4. CONTRACTOR TO TEMPORARILY SEAL ANY HOLES IN TUNNEL AT THE END OF EACH WORK DAY.
5. CONTRACTOR TO REMOVE ALL TOOLS AND EQUIPMENT AT THE END OF EACH WORK DAY.
6. COORDINATE ALL WORK AND SECURITY WITH CCUSO.
7. WORK LIMITS WITHIN CCUSO YARD ARE 8:00 AM - 4:30 PM.
8. SHORE / PROTECT EXISTING FENCES, RAMPS, ETC. AS REQUIRED DURING JOINT REPLACEMENT.



1 ENLARGED GINZBERG BUILDING TUNNEL REPAIR PLANS
1/16" = 1'-0"

TUNNEL REPAIR LEGEND (SEE S1.10 FOR DETAILS)

WATERPROOFING AND INSULATION TO BE INSTALLED FROM STATION 11+30 TO 12+23 AND 12+58 TO 15+06, INCLUDING THE STUB SECTION OF TUNNEL AT THE NORTHWEST OF GINZBERG. EXCAVATION TO STOP AT EXISTING ROADWAY. SEE TUNNEL SECTIONS ON S1.10.

- 1 CRACK INJECTION (WALLS AND LID) (SEE DETAIL 3/S1.10)
- 2 CRACK INJECTION (WALLS ONLY)(SEE 3/S1.10)
- 3 JOINT REPLACEMENT (WALLS AND LID) (SEE DETAIL 4/S1.11)
- PARTIAL DEPTH CONCRETE REPAIR (UNDERSIDE OF LID) (SEE DETAIL 1/S1.10)
- PARTIAL DEPTH CONCRETE REPAIR (INTERIOR OF WALL) (SEE DETAIL 1/S1.10)
- RESURFACING (BOTTOM OF LID) (SEE DETAIL 4/S1.10)
- REPLACE TUNNEL LID (SEE DETAIL 7/S1.10)
- CONDUIT REMOVAL (SEE DETAIL 8/S1.11)

REPLACE NEW SUBDRAINS PER DETAILS ON S1.10 & ELEVATIONS ON CIVIL DRAWINGS RE-USE EXISTING BORED PIPE UNDER ROADWAY. CLEAN PRIOR TO USE.

PROTECT IN PLACE ALL UTILITIES, LIGHT POLES, ETC. AS REQUIRED DURING CONSTRUCTION.

9072.00 DHS CHMHI TUNNEL REPAIRS

GINZBERG BUILDING TUNNEL - ENLARGE PLANS

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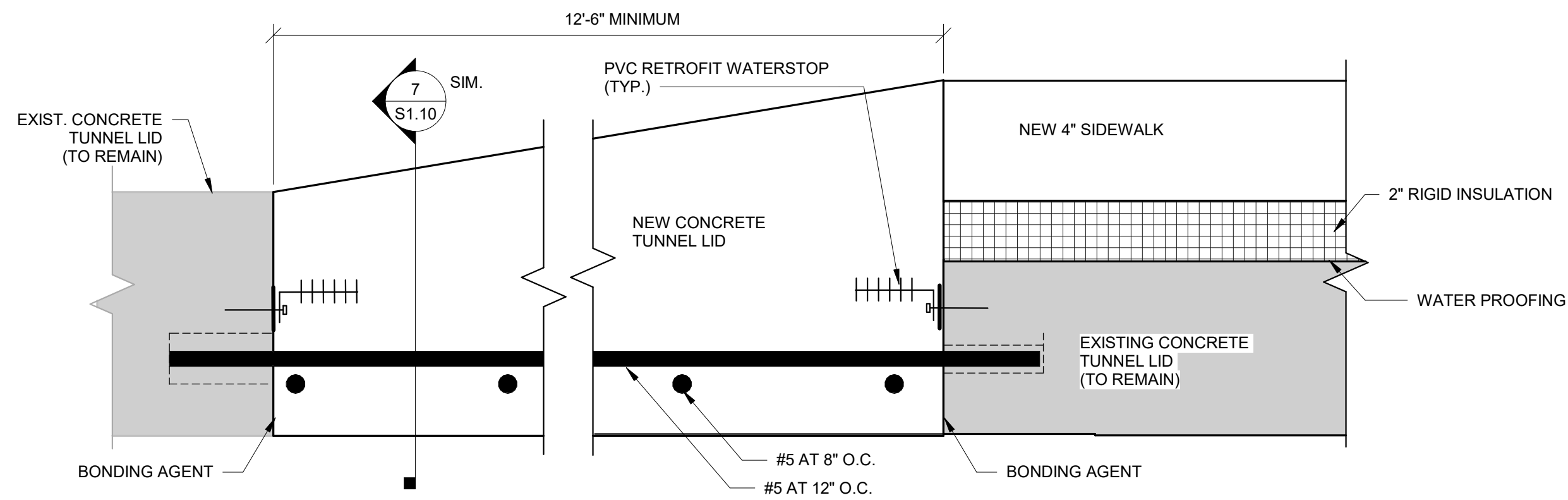
Sheet S1.04

MARK	REVISION	DATE	BY	Scale:	Field Bk:	Pg:
2	ADDENDUM 3	5/24/19				
1	ADDENDUM 1	5/15/19				
Engineer:	BDM	Checked By:	CGG	Scale:		
Technician:	DBT	Date:	4/19/19	Field Bk:		
Project No:	119.0096.03					Sheet S1.04

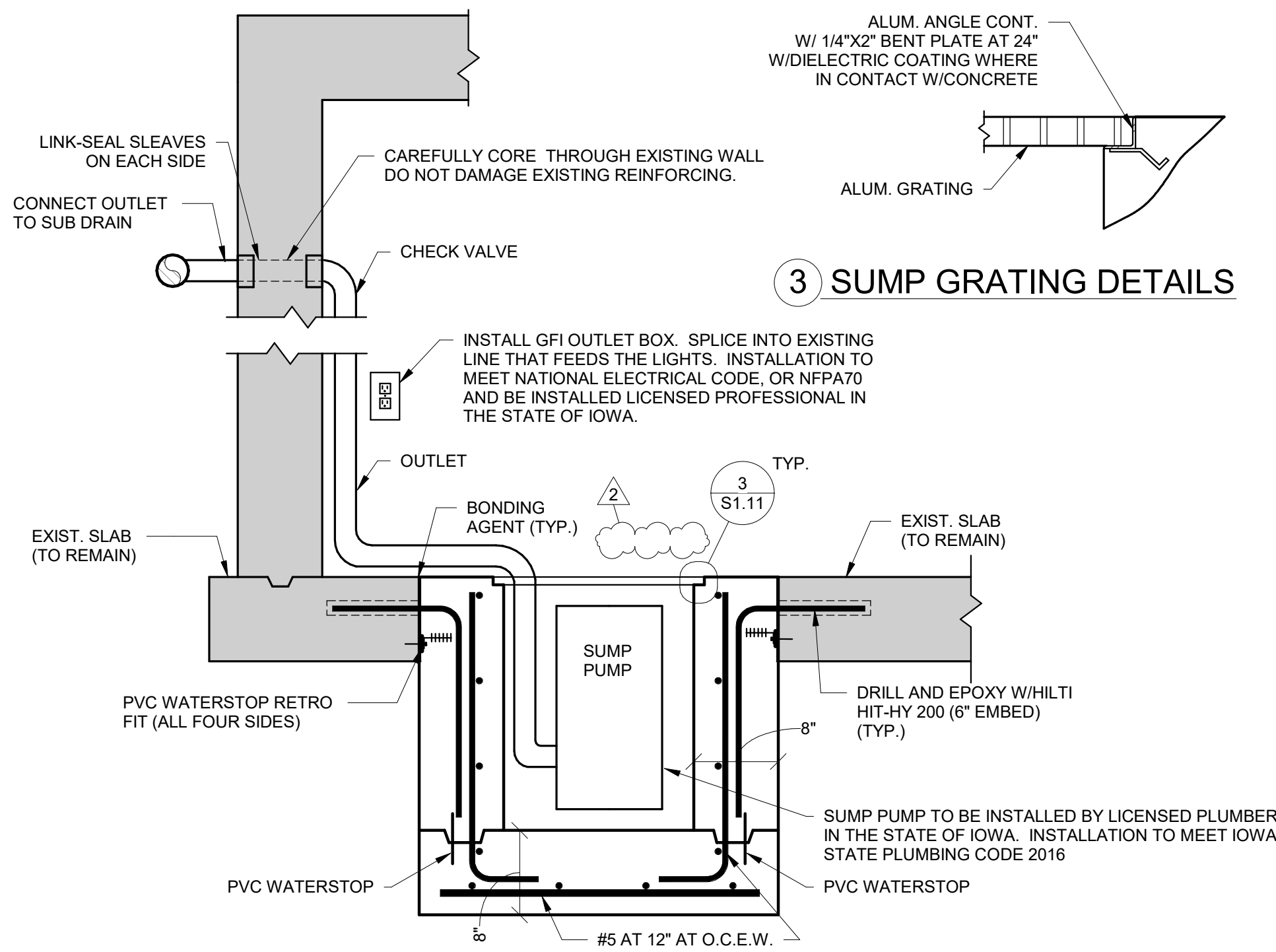
DETAILS

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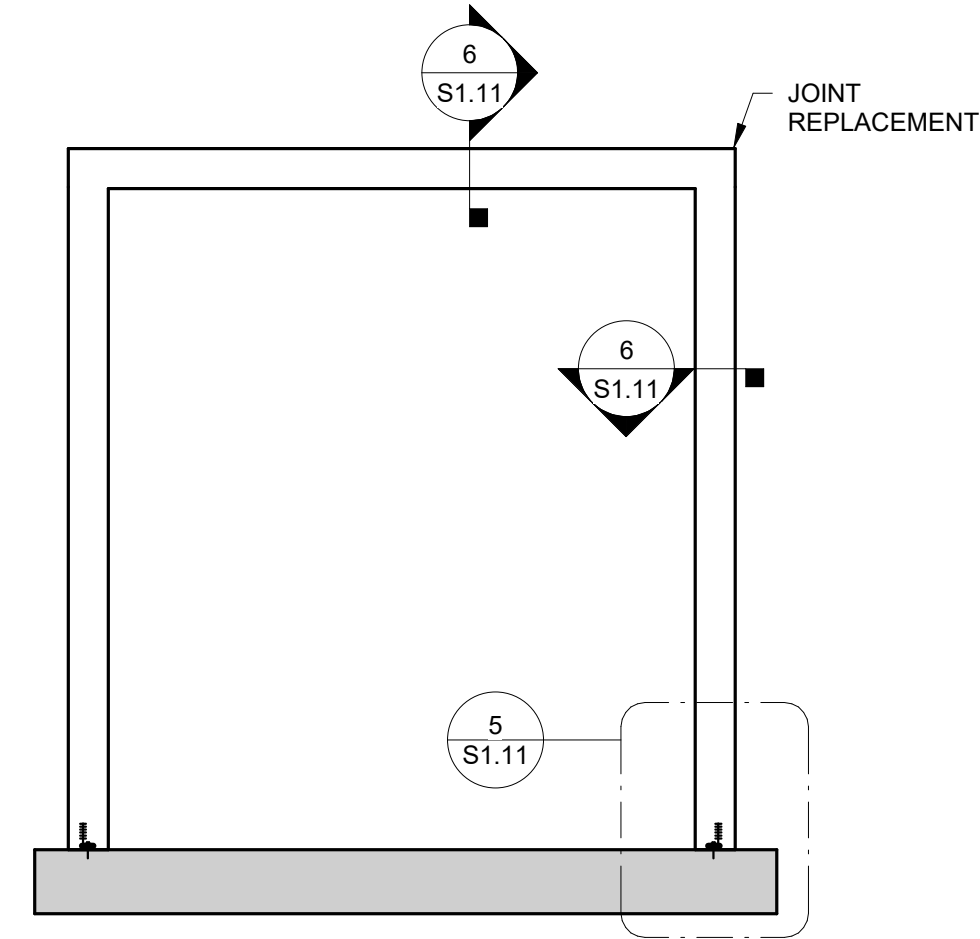
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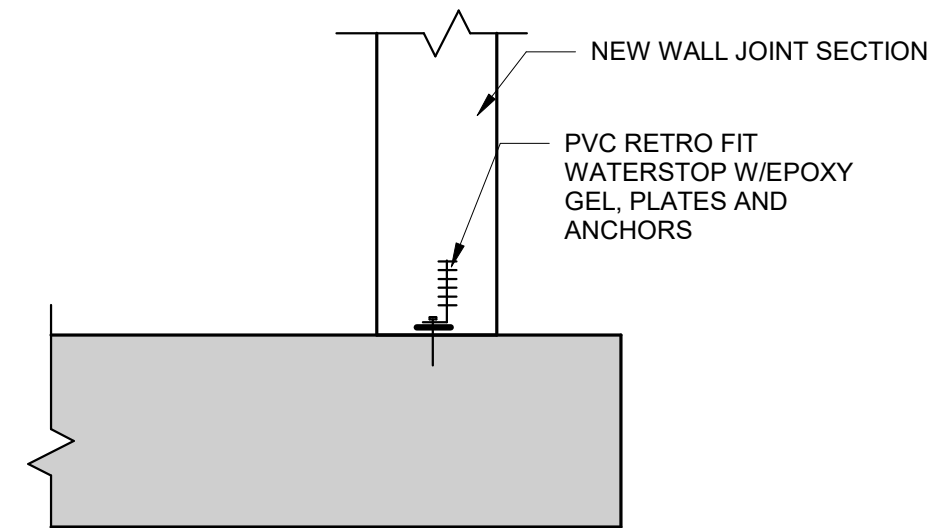
1 CONCRETE TUNNEL LID TRANSITION
3" = 1'-0"



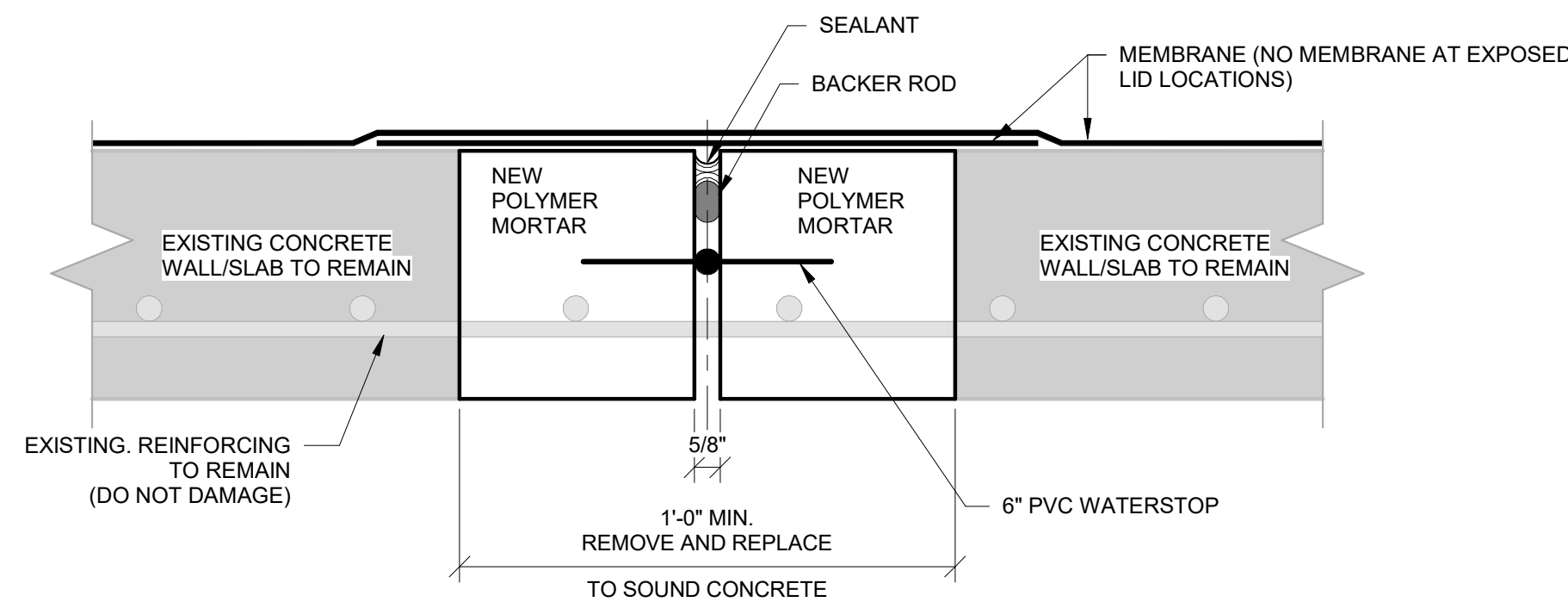
2 TYPICAL SUMP PIT DETAIL
1" = 1'-0"



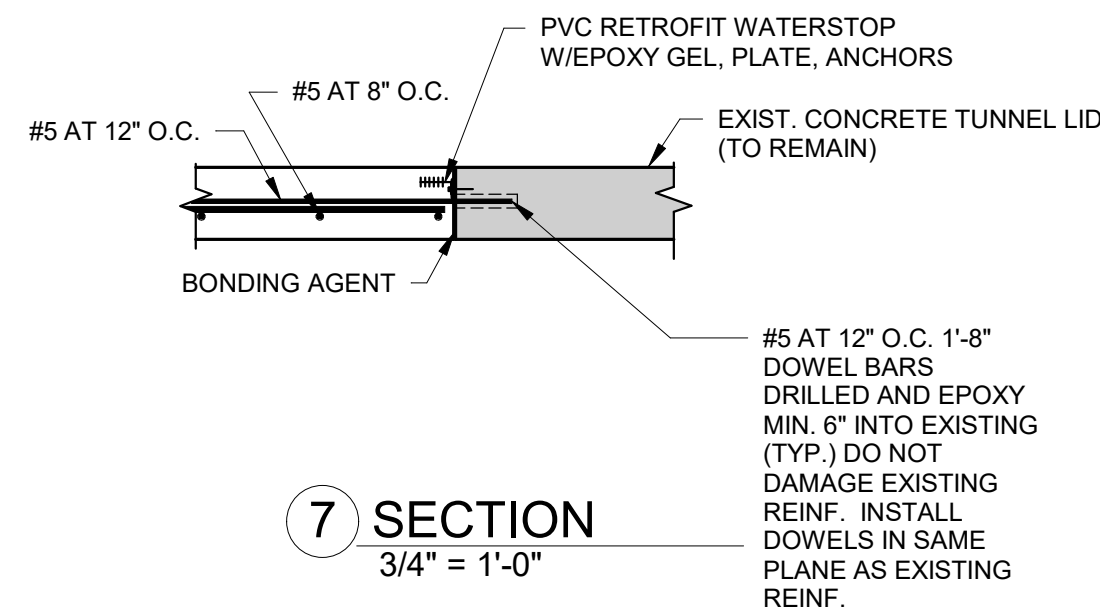
4 JOINT REPLACEMENT
1/2" = 1'-0"



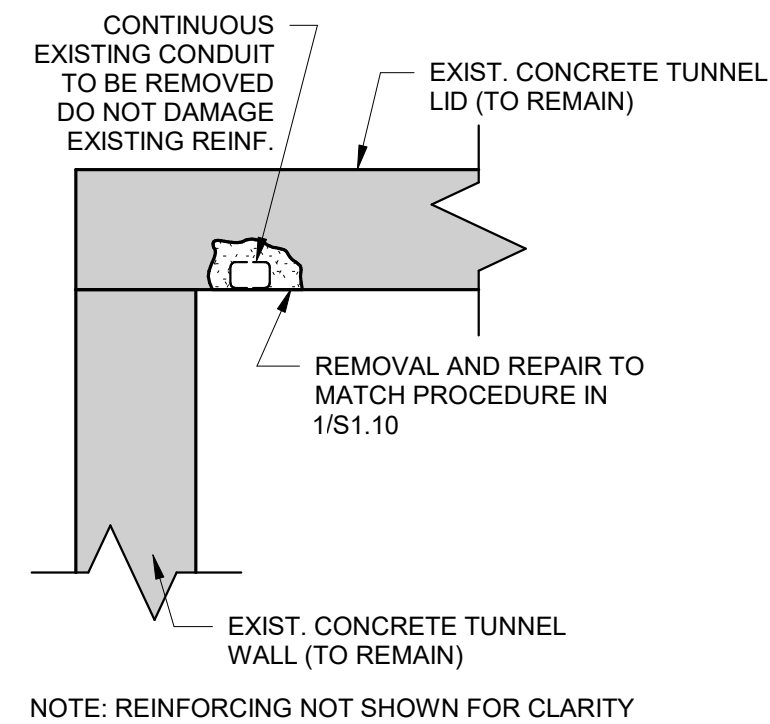
5 SECTION
1 1/2" = 1'-0"



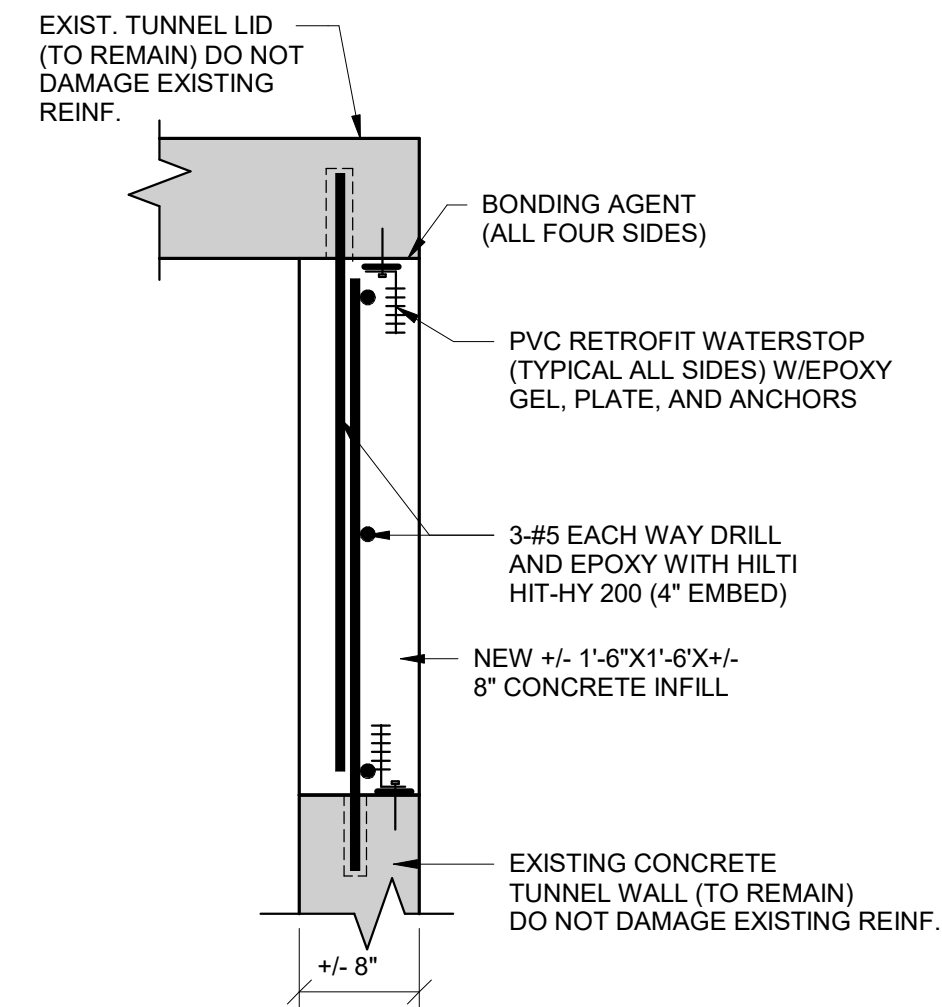
6 SECTION
3" = 1'-0"



7 SECTION
3/4" = 1'-0"



8 TYPICAL CONDUIT REMOVAL DETAIL
1 1/2" = 1'-0"



9 TYPICAL VENT INFILL DETAIL
1 1/2" = 1'-0"

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DETAILS

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Project No: 119.0096.03

Sheet S1.11

Sheet S1.11

MARK	REVISION	DATE	BY
2	ADDENDUM 3	5/24/19	CGG
Engineer:	BDM	Checked By:	CGG
Technician:	DBT	Date:	4/19/19
Field Bk:			
Project No:	119.0096.03		

